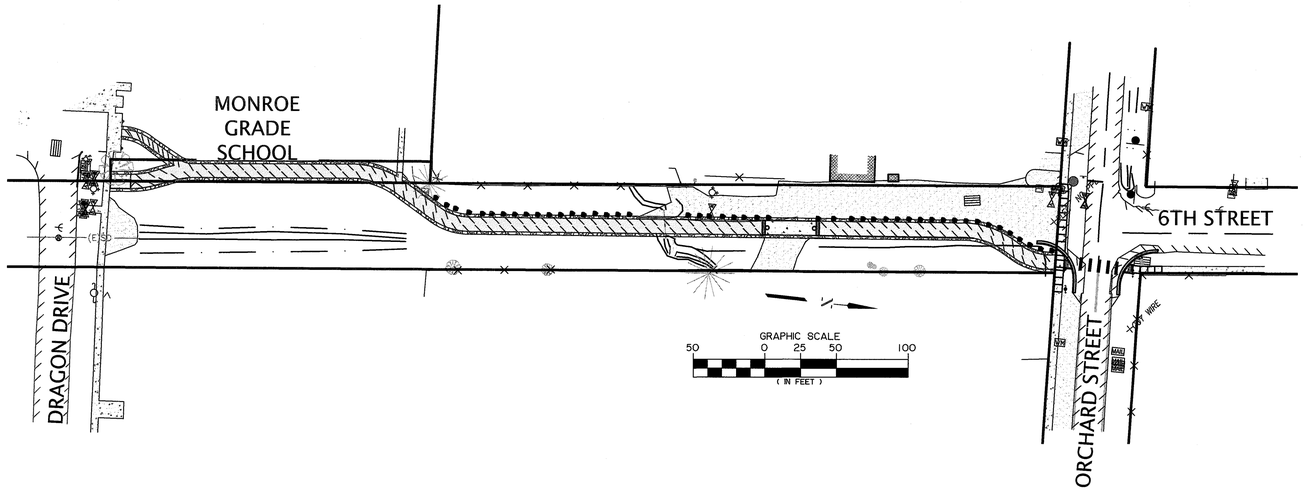
SAFE ROUTES TO SCHOOL
MONROE GRADE SCHOOL
ORCHARD AND 6TH STREET

UTILITY PROVIDERS		
UTILITY	PROVIDER	PHONE NUMBER
WATER	CITY OF MONROE	541-847-5175
SEWER	CITY OF MONROE	541-847-5175
STORM	CITY OF MONROE	541-847-5175
ELECTRIC	PACIFIC POWER	503-255-4634
TELEPHONE	MONROE TELEPHONE CO	541-847-5135



ABBREVIATIONS

TC	TOP OF CURB
GL	GUTTER LINE
C	CONCRETE
AC	ASPHALT CONCRETE
BW	BACK OF WALK
HMAC	HOT MIX ASPHALT
MAX.	MAXIMUM
MIN.	MINIMUM
PSI	POUNDS PER SQUARE INCH
STA.	STATION
HWY.	HIGHWAY
STD.	STANDARD
DWG	DRAWING
W/L	WATERLINE
EX.	EXISTING
PROP.	PROPOSED
SAN	SANITARY
LAT	LATERAL
IE	INVERT ELEVATION
ELEV.	ELEVATION
FG	FINISHED GRADE
EG	EXISTING GRADE
RRFB	RECTANGULAR RAPID FLASHING BEACONS
HORZ.	HORIZONTAL
VERT.	VERTICAL
ODOT	OREGON DEPARTMENT OF TRANSPORTATION
PC	POINT OF CURVATURE
PT	POINT OF TANGENCY
PVI	POINT OF VERTICAL INTERSECTION
LVC	LENGTH OF VERTICAL INTERSECTION
BVCS	BEGIN VERTICAL CURVE STATION
EVCS	END VERTICAL CURVE STATION
BVCE	BEGIN VERTICAL CURVE ELEVATION
EVCE	END VERTICAL CURVE ELEVATION
PCC	POINT OF COMPOUND CURVE
PRC	POINT OF REVERSE CURVE
CL	CENTERLINE
L	LEFT
R	RIGHT
WW	WASTEWATER
SS	SANITARY SEWER
SD	STORM DRAIN
STM	STORM
MH	MANHOLE
CB	CATCH BASIN
DCVA	DOUBLE CHECK VALVE ASSEMBLY

LEGEND

- PROPERTY LINE
- CURB
- EDGE OF ASPHALT
- CONTOUR LINE
- TOP OF DITCH
- BOTTOM OF DITCH
- FENCE
- (E)G (E)G GAS PIPE LINE
- (E)SD (E)SD STORM DRAIN LINE
- (E)W (E)W WATER LINE
- (E)OHW (E)OHW OVER HEAD WIRE
- (E)T (E)T TELEPHONE LINE
- (E)E (E)E UNDERGROUND ELECTRICAL LINE
- DECIDUOUS TREE
- EVERGREEN TREE
- EXISTING CONCRETE
- GRAVEL
- BUILDING
- WM WATER METER
- H2O VLT WATER VAULT
- WATER VALVE
- FIRE HYDRANT
- FIRE DEPARTMENT CONNECTION
- CATCH BASIN
- AREA DRAIN
- SD STORM DRAIN MANHOLE
- CLEAN OUT
- T TELEPHONE RISER
- POWER POLE
- SIGN
- MAIL MAIL BOX
- UNKNOWN UTILITY BOX
- BOLLARD
- ROCK DELINEATION
- SAW CUT
- LIMITS OF DISTURBANCE
- PROPOSED POWER POLE
- PROPOSED CURB
- PROPOSED AC
- PROPOSED CONCRETE
- PROPOSED MULTI USE PATH – AC
- PROPOSED MULTI USE PATH – GRAVEL SHOULDER
- TRUNCATED DOME
- PROPOSED RRFB
- PROPOSED SIGN

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p: 541.746.0637
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REGISTERED PROFESSIONAL ENGINEER
#62780PE
OREGON
JULY 13, 2000
DAMIEN GILBERT
Digitally signed by Damien Gilbert
Date: 2021.05.10 11:49:57 -07'00'
EXPIRES: JUNE 30, 2021

BENTON COUNTY PUBLIC WORKS DEPARTMENT
APPROVED
BY GRK
DATE 5.12.21

SAFE ROUTES TO SCHOOL
MONROE GRADE SCHOOL
CITY OF MONROE, OREGON
PO BOX 486
MONROE, OREGON

revisions:

date: MAY 10, 2021
drawn by: JLL
designer: JLL
project no: 18-005E2

COVER

sheet: **C0**

Z:\2018\18-005E Bailey Branch Trail\Drawings\Branch DWG For SRTS Grant\18-005E2 Safe Routes To School - Orchard And 6th.dwg 5/10/2021 10:58 AM JILLE

REQUIRED TESTING AND FREQUENCY TABLE	PARTY RESPONSIBLE FOR PAYMENT		
	CONTRACTOR		OTHERS (see note 1)
STREETS, PARKING LOTS, PADS, FILLS, ETC			
ASPHALT 1 TEST/6,000 S.F./LIFT (4 MIN.)	X	SEE NOTE 2	
PIPED UTILITIES, ALL			
TRENCH BACKFILL 1 TEST/200 FOOT TRENCH/LIFT (4 MIN.)	X	SEE NOTE 2	
TRENCH AC RESTORATION 1 TEST/300 FOOT OF TRENCH (4 MIN.)	X	SEE NOTE 2	
WATER			
PRESSURE TEST (TO BE WITNESSED BY OWNER'S REPRESENTATIVE OR APPROVING AGENCY)	X	SEE NOTE 4	
BACTERIAL WATER TEST PER OREGON HEALTH DIVISION	X	SEE NOTE 2	
CHLORINE RESIDUAL TEST PER CITY REQUIREMENTS	X	SEE NOTE 2	
SANITARY SEWER (GRAVITY)			
PIPE –AIR OR HYDROSTATIC PER ODOT REQUIREMENTS. –DEFLECTION TESTING PER ODOT REQUIREMENTS. –VIDEO INSPECTION PER ODOT REQUIREMENTS.	X	SEE NOTE 2	
MANHOLES VACUUM TESTING PER ODOT REQUIREMENTS	X	SEE NOTE 2	
CONCRETE			
SLUMP, AIR & CYLINDERS FOR ALL STRUCTURES CURBS, SIDEWALKS AND PCC PAVEMENTS. UNLESS OTHERWISE SPECIFIED, ONE SET OF CYLINDERS PER 100 CUBIC YARDS (OR PORTION THEREOF) OF CONCRETE POURED PER DAY. SLUMP & AIR TESTS REQUIRED ON SAME LOAD AS CYLINDERS.	X	SEE NOTE 2	

NOTE 1: "OTHERS" REFERS TO CITY'S AUTHORIZED REPRESENTATIVE OF APPROVING AGENCY AS APPLICABLE. CONTRACTOR RESPONSIBLE FOR SCHEDULING TESTING. ALL TESTING MUST BE COMPLETED PRIOR TO PERFORMING SUBSEQUENT WORK.

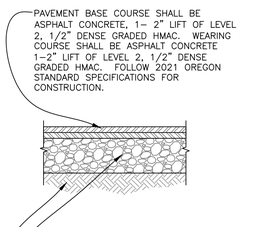
NOTE 2: TESTING MUST BE PERFORMED BY AN APPROVED INDEPENDENT TESTING LABORATORY OR COMPANY.

NOTE 3: IN ADDITION TO IN–PLACE DENSITY TESTING, THE SUBGRADE AND BASE ROCK SHALL BE PROOF ROLLED WITH A LOADED 10 YARD DUMP TRUCK PROVIDED BY THE CONTRACTOR. BASEROCK PROOFROLL SHALL TAKE PLACE IMMEDIATELY PRIOR TO (WITHIN 24 HOURS OF) PAVING, AND SHALL BE WITNESSED BY THE CITY'S AUTHORIZED REPRESENTATIVE OR APPROVING AGENCY. LOCATION AND PATTERN OF PROOFROLL TO BE DIRECTED BY SAID CITY'S REPRESENTATIVE OR APPROVING AGENCY.

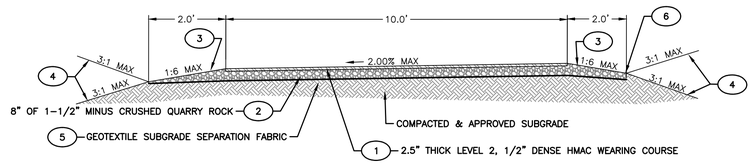
NOTE 4: TO BE WITNESSED BY THE CITY'S REPRESENTATIVE OR APPROVING AGENCY. THE CONTRACTOR SHALL PERFORM PRE–TESTS PRIOR TO SCHEDULING WATERLINE OR SANITARY SEWER PRESSURE TESTS, OR PIPELINE MANDREL TEST.

GENERAL CONSTRUCTION NOTES

1. CONTRACTOR SHALL PROCURE, AND CONFORM TO ALL CONSTRUCTION PERMITS REQUIRED BY THE CITY OF MONROE, BENTON COUNTY AND ODOT.
2. ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952–001–0010 THROUGH OAR 952–001–0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER. (NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS 800–332–2334 or 811).
3. CONTRACTOR TO NOTIFY CITY, COUNTY AND ALL UTILITY COMPANIES A MINIMUM OF 48 BUSINESS HOURS (2 BUSINESS DAYS) PRIOR TO START OF CONSTRUCTION, AND COMPLY WITH ALL OTHER NOTIFICATION REQUIREMENTS OF AGENCIES WITH JURISDICTION OVER THE WORK.
4. CONTRACTOR SHALL PROVIDE ALL BONDS AND INSURANCE REQUIRED BY PUBLIC AND/OR PRIVATE AGENCIES HAVING JURISDICTION. WHERE REQUIRED BY PUBLIC AND/OR PRIVATE AGENCIES HAVING JURISDICTION, THE CONTRACTOR SHALL SUBMIT A SUITABLE MAINTENANCE BOND PRIOR TO FINAL PAYMENT.
5. ALL MATERIALS AND WORKMANSHIP FOR FACILITIES IN STREET RIGHT–OF–WAY OR EASEMENTS SHALL CONFORM TO APPROVING AGENCIES' CONSTRUCTION SPECIFICATIONS WHEREIN EACH HAS JURISDICTION, INCLUDING BUT NOT LIMITED TO THE CITY, COUNTY, OREGON HEALTH DIVISION (OHD) AND THE OREGON DEPARTMENT OF ENVIRONMENTAL QUALITY (DEQ).
6. UNLESS OTHERWISE APPROVED BY THE PUBLIC WORKS DIRECTOR, CONSTRUCTION OF ALL PUBLIC FACILITIES SHALL BE DONE BETWEEN 7:00 A.M. AND 6:00 P.M., MONDAY THROUGH SATURDAY.
7. THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THE PROJECT IN ACCORDANCE WITH THE APPROVED CONSTRUCTION DRAWINGS INCLUDING SUCH INCIDENTALS AS MAY BE NECESSARY TO MEET APPLICABLE AGENCY REQUIREMENTS AND PROVIDE A COMPLETED PROJECT.
8. ANY INSPECTION BY THE CITY, COUNTY OR OTHER AGENCIES SHALL NOT, IN ANY WAY, RELIEVE THE CONTRACTOR FROM ANY OBLIGATION TO PERFORM THE WORK IN STRICT COMPLIANCE WITH THE CONTRACT DOCUMENTS, APPLICABLE CODES, AND AGENCY REQUIREMENTS.
9. CONTRACTOR SHALL MAINTAIN ONE COMPLETE SET OF APPROVED DRAWINGS ON THE CONSTRUCTION SITE AT ALL TIMES WHEREON HE WILL RECORD ALL APPROVED DEVIATIONS IN CONSTRUCTION FROM THE APPROVED DRAWINGS, AS WELL AS THE STATION LOCATIONS AND DEPTHS OF ALL EXISTING UTILITIES ENCOUNTERED. THESE FIELD RECORD DRAWINGS SHALL BE KEPT UP TO DATE AT ALL TIMES AND SHALL BE AVAILABLE FOR INSPECTION BY THE CITY OR OWNER'S REPRESENTATIVE UPON REQUEST. FAILURE TO CONFORM TO THIS REQUIREMENT MAY RESULT IN DELAY IN PAYMENT AND/OR FINAL ACCEPTANCE OF THE PROJECT.
10. UPON COMPLETION OF CONSTRUCTION OF ALL NEW FACILITIES, CONTRACTOR SHALL SUBMIT A CLEAN SET OF FIELD RECORD DRAWINGS CONTAINING ALL AS–BUILT INFORMATION TO THE ENGINEER. ALL INFORMATION SHOWN ON THE CONTRACTOR'S FIELD RECORD DRAWINGS SHALL BE SUBJECT TO VERIFICATION. IF SIGNIFICANT ERRORS OR DEVIATIONS ARE NOTED, AN AS–BUILT SURVEY PREPARED AND STAMPED BY A REGISTERED PROFESSIONAL LAND SURVEYOR SHALL BE COMPLETED AT THE CONTRACTOR'S EXPENSE.
11. CONTRACTOR SHALL PROCURE AND CONFORM TO DEQ STORMWATER PERMIT NO. 1200C FOR CONSTRUCTION ACTIVITIES WHERE 1 ACRE OR MORE ARE DISTURBED.
12. THE CONTRACTOR SHALL RETAIN AND PAY FOR THE SERVICES OF A REGISTERED CIVIL ENGINEER AND/OR LAND SURVEYOR LICENSED IN THE STATE OF OREGON TO ESTABLISH CONSTRUCTION CONTROL AND PERFORM INITIAL CONSTRUCTION SURVEYS TO ESTABLISH THE LINES AND GRADES OF IMPROVEMENTS AS INDICATED ON THE DRAWINGS. STAKING FOR BUILDINGS, STRUCTURES, CURBS, GRAVITY DRAINAGE PIPES/STRUCTURES AND OTHER CRITICAL IMPROVEMENTS SHALL BE COMPLETED USING EQUIPMENT ACCURATE TO 0.04 FEET HORIZONTALLY AND 0.02 FEET VERTICALLY, OR BETTER. USE OF GPS EQUIPMENT FOR CONSTRUCTION STAKING OF THESE IMPROVEMENTS IS ALLOWED IF USED IN CONJUNCTION WITH THE ESTABLISHED CONSTRUCTION CONTROL MENTIONED ABOVE.
13. CONTRACTOR SHALL ERECT AND MAINTAIN BARRICADES, WARNING SIGNS, TRAFFIC CONES PER CITY AND COUNTY REQUIREMENTS IN ACCORDANCE WITH THE MUTCD (INCLUDING OREGON AMENDMENTS). ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES. CONTRACTOR SHALL COORDINATE WITH PROPERTY OWNERS AND/OR RESIDENTS REGARDING ACCESS DURING CONSTRUCTION. ALL TRAFFIC CONTROL MEASURES SHALL BE APPROVED AND IN PLACE PRIOR TO ANY CONSTRUCTION ACTIVITY. PRIOR TO ANY WORK IN THE EXISTING PUBLIC RIGHT–OF–WAY, CONTRACTOR SHALL SUBMIT FINAL TRAFFIC CONTROL PLAN TO THE CITY, COUNTY AND ODOT FOR REVIEW AND ISSUANCE OF A LANE CLOSURE OR WORK IN RIGHT–OF–WAY PERMIT
14. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT ALL REQUIRED OR NECESSARY INSPECTIONS ARE COMPLETED BY AUTHORIZED INSPECTORS PRIOR TO PROCEEDING WITH SUBSEQUENT WORK WHICH COVERS OR THAT IS DEPENDENT ON THE WORK TO BE INSPECTED. FAILURE TO OBTAIN NECESSARY INSPECTION(S) AND APPROVAL(S) SHALL RESULT IN THE CONTRACTOR BEING FULLY RESPONSIBLE FOR ALL PROBLEMS ARISING FROM UNINSPECTED WORK.
15. UNLESS OTHERWISE SPECIFIED, THE ATTACHED "REQUIRED TESTING AND FREQUENCY" TABLE OUTLINES THE MINIMUM TESTING SCHEDULE FOR THE PROJECT. THIS TESTING SCHEDULE IS NOT COMPLETE, AND DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF OBTAINING ALL NECESSARY INSPECTIONS OR OBSERVATIONS FOR ALL WORK PERFORMED, REGARDLESS OF WHO IS RESPONSIBLE FOR PAYMENT. COST FOR RETESTING SHALL BE BORNE BY THE CONTRACTOR.
16. THE LOCATION AND DESCRIPTIONS OF EXISTING UTILITIES SHOWN ON THE DRAWINGS ARE COMPILED FROM AVAILABLE RECORDS AND/OR FIELD SURVEYS. THE ENGINEER OR UTILITY COMPANIES DO NOT GUARANTEE THE ACCURACY OR THE COMPLETENESS OF SUCH RECORDS. CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND SIZES OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MARKING ALL EXISTING SURVEY MONUMENTS OF RECORD (INCLUDING BUT NOT LIMITED TO PROPERTY AND STREET MONUMENTS) PRIOR TO CONSTRUCTION. IF ANY SURVEY MONUMENTS ARE REMOVED, DISTURBED OR DESTROYED DURING CONSTRUCTION OF THE PROJECT, THE CONTRACTOR SHALL RETAIN AND PAY FOR THE SERVICES OF A REGISTERED PROFESSIONAL SURVEYOR LICENSED IN THE STATE OF OREGON TO REFERENCE AND REPLACE ALL SUCH MONUMENTS PRIOR TO FINAL PAYMENT. THE MONUMENTS SHALL BE REPLACED WITHIN A MAXIMUM OF 90 DAYS, AND THE COUNTY SURVEYOR SHALL BE NOTIFIED IN WRITING AS REQUIRED BY PER ORS 209.150.
18. ALL FACILITIES SHALL BE MAINTAINED IN–PLACE BY THE CONTRACTOR UNLESS OTHERWISE SHOWN OR DIRECTED. CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO SUPPORT, MAINTAIN, OR OTHERWISE PROTECT EXISTING UTILITIES AND OTHER FACILITIES AT ALL TIMES DURING CONSTRUCTION. CONTRACTOR TO LEAVE EXISTING FACILITIES IN AN EQUAL OR BETTER–THAN–ORIGINAL CONDITION AND TO THE SATISFACTION OF THE CITY AND OWNER'S REPRESENTATIVE.
19. UTILITIES OR INTERFERING PORTIONS OF UTILITIES THAT ARE ABANDONED IN PLACE SHALL BE REMOVED BY THE CONTRACTOR TO THE EXTENT NECESSARY TO ACCOMPLISH THE WORK. THE CONTRACTOR SHALL PLUG THE REMAINING EXPOSED ENDS OF ABANDONED UTILITIES.
20. CONTRACTOR SHALL REMOVE ALL EXISTING SIGNS, MAILBOXES, FENCES, LANDSCAPING, ETC., AS REQUIRED TO AVOID DAMAGE DURING CONSTRUCTION AND REPLACE THEM TO EXISTING OR BETTER CONDITION.
21. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MANAGING CONSTRUCTION ACTIVITIES TO ENSURE THAT PUBLIC STREETS AND RIGHT–OF–WAYS ARE KEPT CLEAN OF MUD, AND DUST OR DEBRIS. DUST ABATEMENT SHALL BE MAINTAINED BY ADEQUATE WATERING OF THE SITE BY THE CONTRACTOR.
22. FINISH PAVEMENT GRADES AT TRANSITION TO EXISTING PAVEMENT SHALL MATCH EXISTING PAVEMENT GRADES OR BE FEATHERED PAST JOINTS WITH PAVEMENT AS REQUIRED TO PROVIDE A SMOOTH, FREE DRAINING SURFACE.
23. ALL EXISTING OR CONSTRUCTED MANHOLES, CLEANOUTS, MONUMENT BOXES, GAS VALVES, WATER VALVES AND SIMILAR STRUCTURES SHALL BE ADJUSTED TO MATCH FINISH GRADE OF THE PAVEMENT, SIDEWALK, LANDSCAPED AREA OR MEDIAN STRIP WHEREIN THEY LIE. VERIFY THAT ALL VALVE BOXES AND RISERS ARE CLEAN AND CENTERED OVER THE OPERATING NUT.
24. CONTRACTOR SHALL SEED AND MULCH (UNIFORMLY BY HAND OR HYDROSEED) EXPOSED SLOPES AND DISTURBED AREAS WHICH ARE NOT SCHEDULED TO BE LANDSCAPED, INCLUDING TRENCH RESTORATION AREAS. IF THE CONTRACTOR FAILS TO APPLY SEED AND MULCH IN A TIMELY MANNER DURING PERIODS FAVORABLE FOR GERMINATION, OR IF THE SEEDED AREAS FAIL TO GERMINATE, THE CITY'S REPRESENTATIVE MAY (AT HIS DISCRETION) REQUIRE THE CONTRACTOR TO INSTALL SOD TO COVER SUCH DISTURBED AREAS.
25. THE CONTRACTOR SHALL HAVE APPROPRIATE EQUIPMENT ON SITE TO PRODUCE A FIRM, SMOOTH, UNDISTURBED SUBGRADE AT THE TRENCH BOTTOM, TRUE TO GRADE. THE BOTTOM OF THE TRENCH EXCAVATION SHALL BE SMOOTH, FREE OF LOOSE MATERIALS OR TOOTH GROOVES FOR THE ENTIRE WIDTH OF THE TRENCH PRIOR TO PLACING THE GRANULAR BEDDING MATERIAL.



PAVEMENT REPAIR DETAIL



MULTI USE PATH – TYPICAL SECTION

STA 11+89.15 TO STA 18+50.44
(NOT TO SCALE)

CONSTRUCTION NOTES

1. PAVEMENT SECTION SHALL BE ONE 2.5" LIFT OF LEVEL 2, 1/2" DENSE GRADED HMAC. FOLLOW 2021 OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION.
2. BASE ROCK SHALL BE 8" MIN. 1–1/2"–0" CRUSHED QUARRY ROCK AGGREGATE. AGGREGATE SHALL BE COMPACTED TO 95% RELATIVE MAXIMUM DENSITY AS DETERMINED BY AASHTO T–180. FOLLOW 2021 OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION.
3. SHOULDER ROCK SHALL BE 1–1/2"–0" CRUSHED QUARRY ROCK AGGREGATE. AGGREGATE SHALL BE COMPACTED TO 95% AT 1:6 MAX SLOPE GRADED FROM BOTH EDGES OF MULTI USE PATH FOR 2'.
4. RESTORE SURFACING TO ORIGINAL OR BETTER CONDITION. COORDINATE WITH CITY OR OWNER FOR LANDSCAPE RESTORATION.
5. GEOTEXTILE SUBGRADE SEPARATION FABRIC TO BE PROPEX GEO–SOLUTIONS GEOTEX 200ST.
6. INSTALL CLASS 200 RIPRAP ALONG EDGE OF MULTI–USE PATH EVERY 8' ON CENTER. EMBED BOTTOM 4"–6" OF CLASS 200 RIPRAP.

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MAY 13, 2005
DAMIEN GILBERT

EXPIRES: JUNE 30, 2021

SAFE ROUTES TO SCHOOL
MONROE GRADE SCHOOL

CITY OF MONROE, OREGON
PO BOX 486
MONROE, OREGON

revisions:

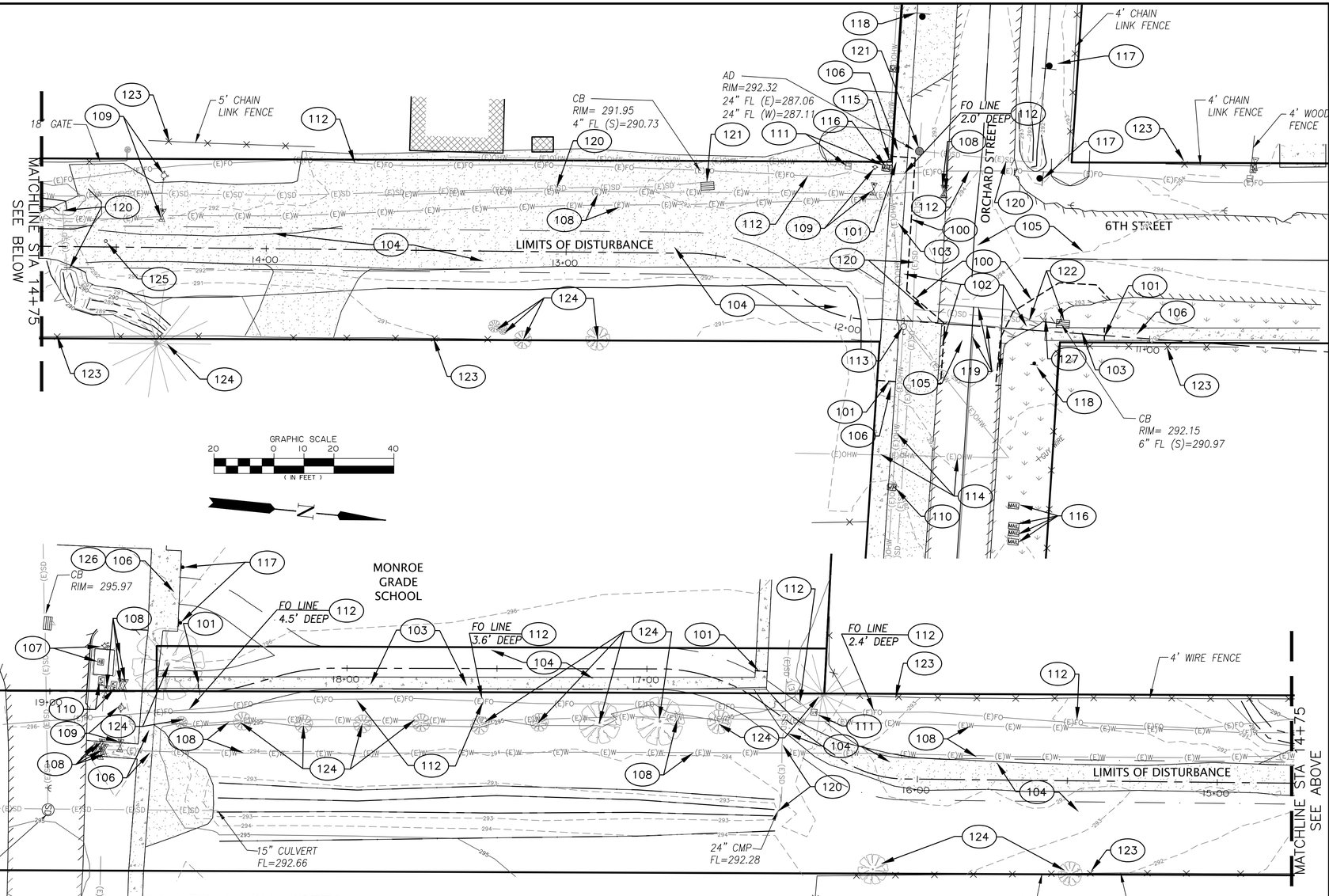
date:	MAY 10, 2021
drawn by:	JLL
designer:	JLL
project no:	18-005E2

TYPICAL SECTION & GENERAL NOTES

sheet: **C1**

NOTES:

- A BENTON COUNTY PERMIT TO PERFORM WORK IN THE COUNTY RIGHT OF WAY IS REQUIRED PRIOR TO ALL WORK WITHIN THE ORCHARD STREET RIGHT OF WAY.
- ALL EXISTING ASPHALT MUST BE SAWCUT PRIOR TO PAVEMENT REPAIR WITH NEAT, STRAIGHT, CLEAN EDGES.



CONSTRUCTION NOTES

100 — SAWCUT EXISTING ASPHALT PAVEMENT WITH NEAT, STRAIGHT, CLEAN EDGES. PROTECT SAWCUT EDGE FROM DAMAGE.

101 — SAWCUT EXISTING CONCRETE SIDEWALK WITH NEAT, STRAIGHT, CLEAN EDGES. PROTECT SAWCUT EDGE FROM DAMAGE.

102 — REMOVE ASPHALT, BASE ROCK, AND ANY SUBGRADE NECESSARY FOR NEW CURB AND GUTTER AND PAVEMENT REPAIR SECTION. SEE SHEET C1 & C6.

103 — REMOVE CONCRETE SIDEWALK, BASE ROCK, AND ANY SUBGRADE NECESSARY FOR NEW CONCRETE SIDEWALK, DRIVEWAY, AND/OR ADA RAMPS. SEE TYPICAL SECTION, SHEET C1 & DETAILS SHEET C7–C10.

104 — REMOVE SURFACING, BASE ROCK, AND ANY SUBGRADE NECESSARY FOR NEW MULTI–USE PATH AND CONCRETE DRIVEWAY PER PROPOSED IMPROVEMENT PLANS, SHEET C3 & C4. SEE DETAILS SHEET C7–C10 & TYPICAL SECTION, SHEET C1.

105 — PROTECT EXISTING ASPHALT PAVEMENT.

106 — PROTECT EXISTING CONCRETE SIDEWALK.

107 — PROTECT EXISTING WATER VAULT AND FDC.

108 — PROTECT EXISTING WATER MAIN AND/OR WATER VALVE.

109 — PROTECT EXISTING FIRE HYDRANT AND VALVES.

110 — PROTECT EXISTING WATER METER.

111 — PROTECT EXISTING COMMUNICATION RISER AND/OR VAULT.

112 — PROTECT EXISTING COMMUNICATION AND/OR FIBER OPTICS LINES.

113 — RELOCATE EXISTING STREET LIGHT SHOWN ON PROPOSED IMPROVEMENT SHEETS C3 & C5. COORDINATE WORK WITH PP&L 1–888–221–7070.

114 — PROTECT EXISTING OVERHEAD POWER LINES.

115 — PROTECT EXISTING POWER POLE AND/OR ANCHOR WIRE.

116 — PROTECT EXISTING MAILBOX(ES).

117 — PROTECT EXISTING SIGN.

118 — REMOVE EXISTING SIGN AND POST PER SIGN & STRIPING PLANS, SHEET C5. RETURN SIGN & POST TO BENTON COUNTY SIGN SHOP.

119 — REMOVE EXISTING STRIPING PER SIGN & STRIPING PLANS, SHEET C5.

120 — PROTECT EXISTING STORM LINE AND/OR STORM CULVERT.

121 — PROTECT EXISTING AREA DRAIN AND/OR CATCH BASIN.

122 — REMOVE EXISTING CATCH BASIN AND PROTECT EXISTING STORM LINE(S) FOR CONNECTION TO NEW CATCH BASIN PER PROPOSED IMPROVEMENT PLANS, SHEET C3 & C6.

123 — PROTECT EXISTING FENCE AND/OR GATE.

124 — PROTECT EXISTING TREE AND ROOT ZONE FROM DAMAGE.

125 — REMOVE EXISTING BOLLARD.

126 — PROTECT EXISTING BIKE RACKS.

127 — PROTECT UNKNOWN UTILITY BOX AND ADJUST TO FINISH GRADE.

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DIGITALLY SIGNED
EXPIRES: JUNE 30, 2021

SAFE ROUTES TO SCHOOL
MONROE GRADE SCHOOL

CITY OF MONROE, OREGON
PO BOX 486
MONROE, OREGON

revisions:

date:	MAY 10, 2021
drawn by:	JLL
designer:	JLL
project no:	18-005E2

EXISTING CONDITIONS & DEMO PLAN

sheet: **C2**

GRAPHIC SCALE
20 0 10 20 40
(IN FEET)

N

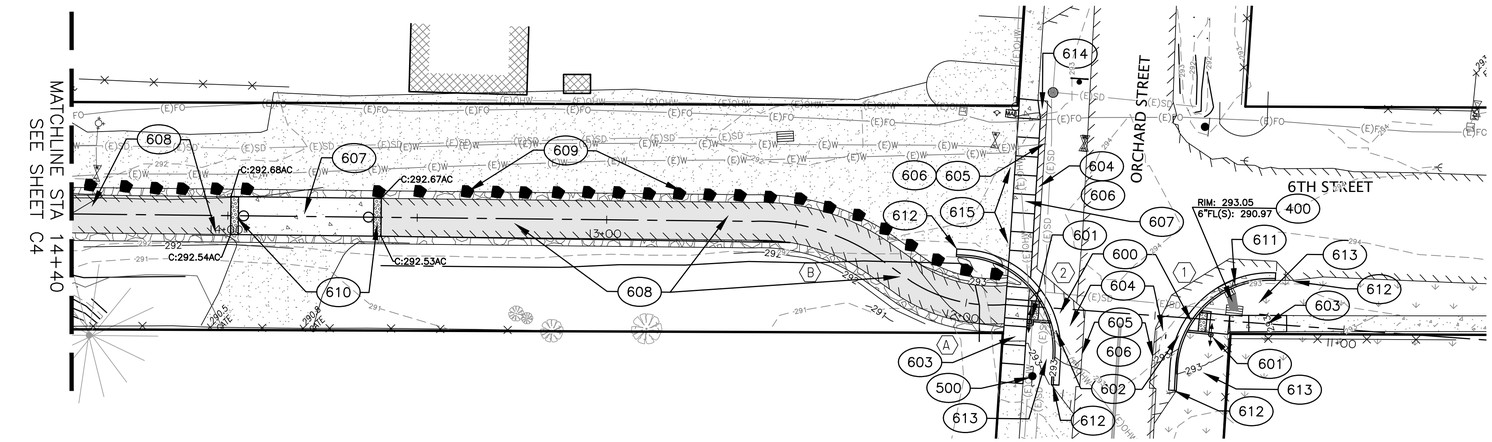
① CURB RETURN DATA	
Δ	90.00
R	25.00'
L	39.27'
P.C.	11+19.03 12.09'RT
P.T.	11+43.55 13.37'LT
CORD LENGTH	35.36'
CORD BEARING	N47°46'22.69" W

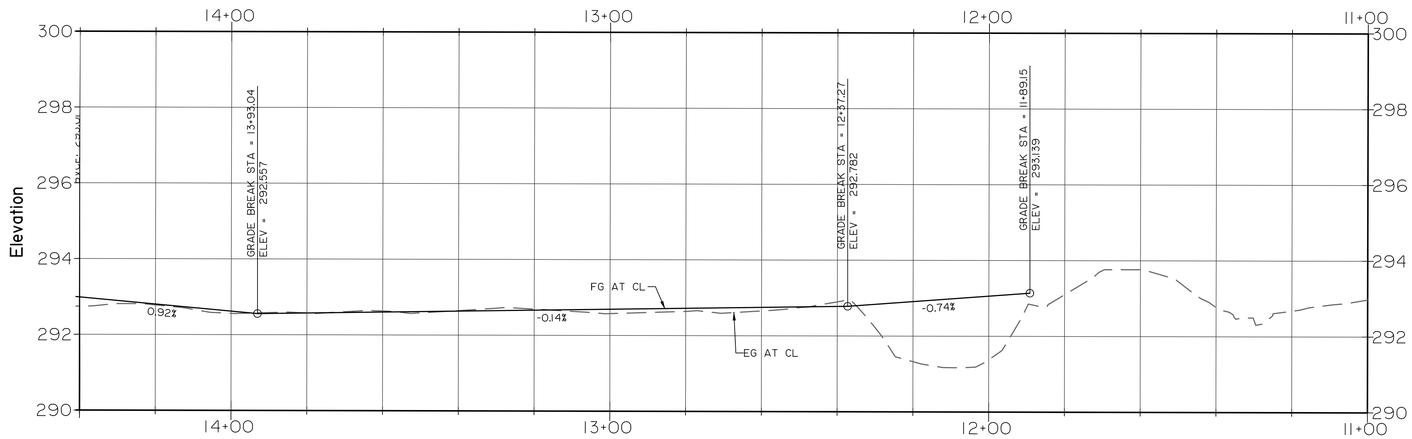
② CURB RETURN DATA	
Δ	89.7543
R	25.05'
L	39.25'
P.C.	11+75.58 12.35'LT
P.T.	12+03.33 12.25'RT
CORD LENGTH	35.35'
CORD BEARING	S42°19'51.47" W

Ⓐ CENTERLINE CURVE DATA	
Δ	37.179I
R	40.00'
L	25.96'
P.C.	11+89.56
P.T.	12+15.52
CORD LENGTH	25.50'
CORD BEARING	N07°08'28.46" E

Ⓑ CENTERLINE CURVE DATA	
Δ	36.622
R	55.00'
L	35.16'
P.C.	12+26.29
P.T.	12+61.44
CORD LENGTH	34.56'
CORD BEARING	N10°20'22.16" E

NOTES:
- A BENTON COUNTY PERMIT TO PERFORM WORK IN THE COUNTY RIGHT OF WAY IS REQUIRED PRIOR TO ALL WORK WITHIN THE ORCHARD STREET RIGHT OF WAY.
- ALL EXISTING ASPHALT MUST BE SAWCUT PRIOR TO PAVEMENT REPAIR WITH NEAT, STRAIGHT, CLEAN EDGES.





6TH STREET MULTI-USE PATH IMPROVEMENTS
SCALE: HORZ: 1" = 20'
VERT: 1" = 2'

CONSTRUCTION NOTES

(400) CONSTRUCT G-2MA CONCRETE CATCH BASIN PER ODOT STD DWG RD 364, SHEET C7, WITH MIN. 12" SUMP. SEE PLAN VIEW FOR RIM AND PIPE ELEVATIONS. CONNECT EXISTING PIPE TO NEW CATCH BASIN USING APPROPRIATE FITTINGS AND COUPLINGS .

(500) RELOCATE POWER POLE. COORDINATE WORK WITH PACIFIC POWER (EDDIE STEINER 541-967-6146).

(600) CONSTRUCT CURB RETURN WITH ADA RAMPS INCLUDING TRUNCATED DOME, PER ODOT STD DWG RD 902, SHEET C8. PLACE 4" MINIMUM THICKNESS OF 1-1/2"-0" CRUSHED QUARRY ROCK. SEE SHEET C6 FOR CURB RETURN DETAILS WITH DIMENSIONS AND SPOT ELEVATIONS.

(601) CONSTRUCT RRFB PEDESTRIAN CROSSING SIGNAL, TAPCO MODEL # 2180-RRFB XLS. SEE RRFB SIGNAL INSTALLATION PLAN, SHEET C5.

(602) CONSTRUCT STANDARD 24" CONCRETE CURB & GUTTER PER ODOT STD DWG RD700, SHEET C7. COUNTER SLOPE SHALL BE MAXIMUM 4% OR LESS IN FRONT OF RAMP.

(603) CONSTRUCT SEPERATED CONCRETE SIDEWALK 4" THICK PER OREGON STANDARD DRAWING RD721, SHEET C8, OVER 4" OF 1-1/2"-0" CRUSHED QUARRY ROCK. SEE SHEET C6 DIMENSIONS AND SPOT ELEVATIONS.

(604) CONSTRUCT AC REPAIR BY PLACING 4" OF COMPACTED LEVEL 2- 1/2" DENSE HMAC OR MATCH EXISTING THICKNESS (WHICHEVER IS GREATER) OVER COMPACTED CRUSHED ROCK PER PAVEMENT REPAIR DETAIL SHEET C1.

(605) MATCH EXISTING PAVEMENT GRADES.

(606) SEAL PAVEMENT JOINT. TACK COAT EXISTING PAVEMENT EDGES. THE MATCHLINE TO EXISTING PAVING SHALL COMPLY WITH ODOT STD DWG RD302, SHEET C7.

(607) CONSTRUCT CONCRETE DRIVEWAY PER ODOT DETAIL DET1740, SHEET C8. PLACE 6" MINIMUM THICKNESS OF 1-1/2"-0 CRUSHED QUARRY ROCK.

(608) CONSTRUCT SHARED USE PATH PER SECTION TYPICAL SECTION DETAIL, SEE SHEET C1.

(609) INSTALL CLASS 200 RIPRAP ALONG EDGE OF MULTI-USE PATH EVERY 8' ON CENTER. EMBED BOTTOM 4"-6" OF CLASS 200 RIPRAP. SEE TYPICAL SECTION DETAIL, SHEET C1 FOR PLACEMENT ALONG EDGE OF MULTI-USE PATH.

(610) INSTALL TRUNCATED DOMES, PER ODOT STD DWG RD902, AND BOLLARDS, PER ODOT STD DWG RD130, SHEET C7, AT NORTH AND SOUTH EDGE OF DRIVEWAY.

(611) CONSTRUCT CONCRETE CURB CUT SPILLWAY PER DETAIL 1, SHEET C10.

(612) CONSTRUCT CONCRETE CURB ENDING PER ODOT STD DETAIL RD700, SHEET C7.

(613) FILL IN BEHIND CONCRETE CURB. RESTORE SURFACING TO ORIGINAL OR BETTER CONDITION. COORDINATE WITH CITY OR OWNER FOR LANDSCAPE RESTORATION.

(614) PROTECT GUY WIRE ANCHOR DURING CONSTRUCTION.

(615) FILL IN BEHIND CONCRETE DRIVEWAY WITH 1-1/2"-0" CRUSHED QUARRY ROCK AGGREGATE. AGGREGATE SHALL BE COMPACTED TO 95% AT 1:6 MAX SLOPE GRADED FROM BACK EDGE OF CONCRETE DRIVEWAY TO MEET EXISTING GRADE.

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JULY 13, 2005
DAMIEN GILBERT
EXPIRES: JUNE 30, 2021

SAFE ROUTES TO SCHOOL MONROE GRADE SCHOOL

CITY OF MONROE, OREGON
PO BOX 486
MONROE, OREGON

revisions:

date: MAY 10, 2021
drawn by: JLL
designer: JLL
project no: 18-005E2

PROPOSED IMPROVEMENTS STA. 11+00 -14+40

sheet: C3

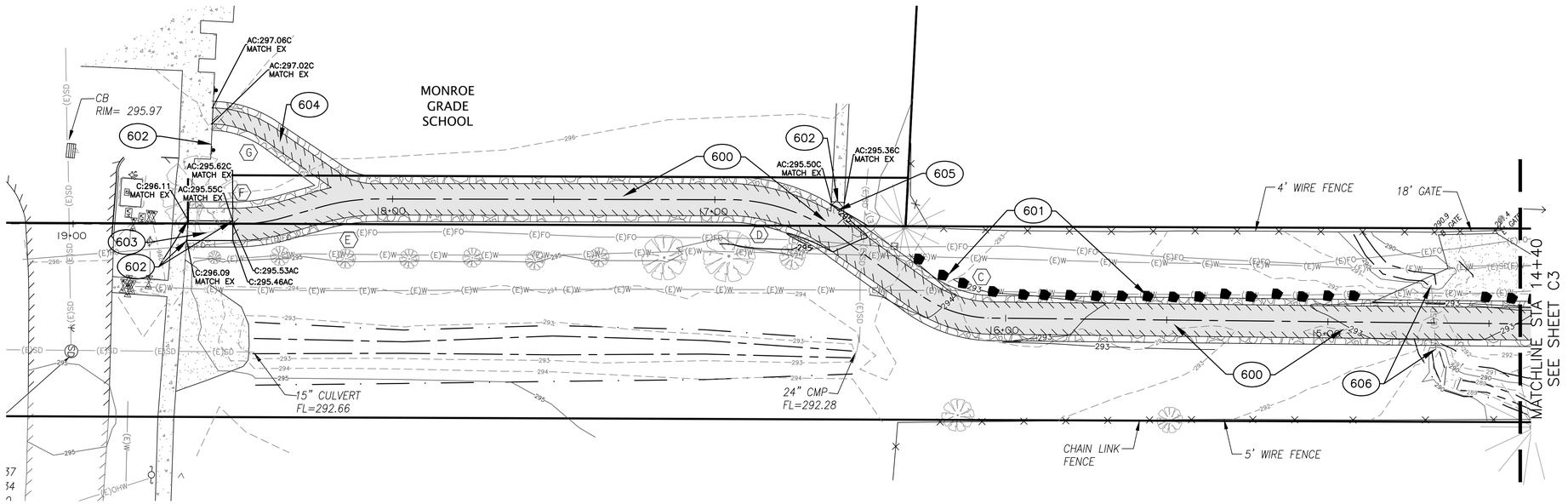
Ⓒ CENTERLINE CURVE DATA	
Δ	32.0325
R	40.00'
L	22.36'
P.C.	16+05.43
P.T.	16+27.79
CORD LENGTH	22.07'
CORD BEARING	S1247'21.01" W

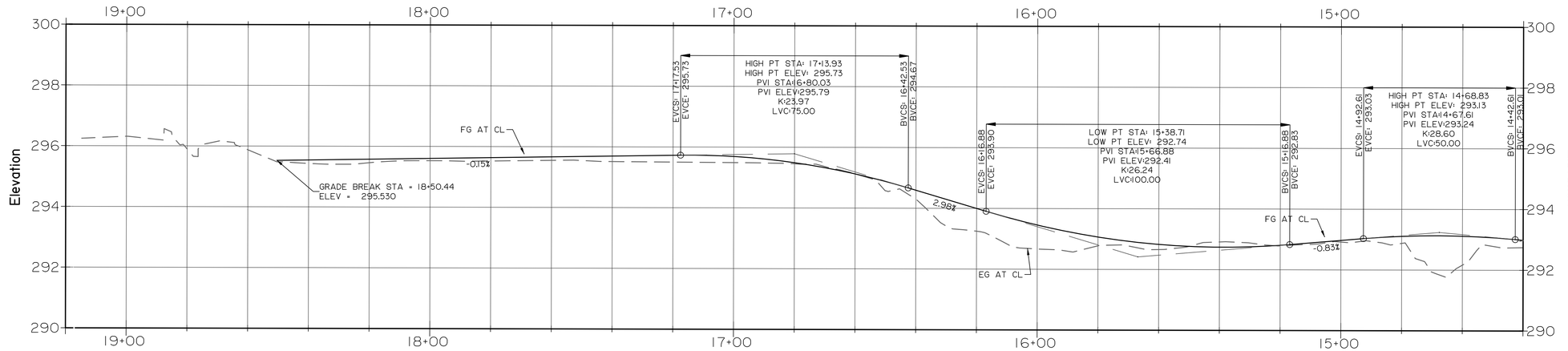
Ⓓ CENTERLINE CURVE DATA	
Δ	34.6364
R	60.00'
L	36.27'
P.C.	16+66.68
P.T.	16+93.33
CORD LENGTH	35.87'
CORD BEARING	S1427'36.51" W

Ⓔ CENTERLINE CURVE DATA	
Δ	17.6569
R	65.00'
L	20.03'
P.C.	18+05.37
P.T.	18+25.35
CORD LENGTH	19.91'
CORD BEARING	S1529'20.12" E

Ⓕ CENTERLINE CURVE DATA	
Δ	14.9059
R	60.00'
L	15.61'
P.C.	18+36.57
P.T.	18+52.18
CORD LENGTH	15.57'
CORD BEARING	N1140'27.88" E

Ⓖ CENTERLINE CURVE DATA	
Δ	31.5268
R	35.00'
L	19.25'
P.C.	18+30.18 24.2'R
P.T.	18+54.83 33.6'R
CORD LENGTH	19.02'
CORD BEARING	N1545'48.22" E





6TH STREET MULTI-USE PATH IMPROVEMENTS

SCALE: HORZ: 1" = 20'
VERT: 1" = 2'

CONSTRUCTION NOTES

600 — CONSTRUCT SHARED USE PATH PER SECTION TYPICAL SECTION DETAIL, SEE SHEET C1.

601 — INSTALL CLASS 200 RIPRAP ALONG EDGE OF MULTI-USE PATH EVERY 8' ON CENTER. EMBED BOTTOM 4"-6" OF CLASS 200 RIPRAP. SEE TYPICAL SECTION DETAIL, SHEET C1 FOR PLACEMENT ALONG EDGE OF MULTI-USE PATH.

602 — MATCH EXISTING CONCRETE GRADES.

603 — CONSTRUCT 6' WIDE CONCRETE SIDEWALK ALONG EXISTING CONCRETE SIDEWALK, PER CONCRETE SIDEWALK DETAIL 2, SHEET C10, TO MATCH END OF MULTI-USE PATH WIDTH AND GRADES.

604 — CONSTRUCT 5' WIDE ASPHALT PATH TO MATCH INTO EXISTING CONCRETE SIDEWALK. PER TYPICAL SECTION DETAIL, SHEET C1, WIDTH OF PATH TO BE ONLY 5' OF AC.

605 — CONSTRUCT 4' WIDE ASPHALT PATH TO MATCH INTO EXISTING CONCRETE SIDEWALK. PER TYPICAL SECTION DETAIL, SHEET C1, WIDTH OF PATH TO BE ONLY 4' OF AC.

606 — PROTECT 24" STORM CULVERT ENDS AND SIDE SLOPE BOTTOM OF DRAINAGE WAGE.

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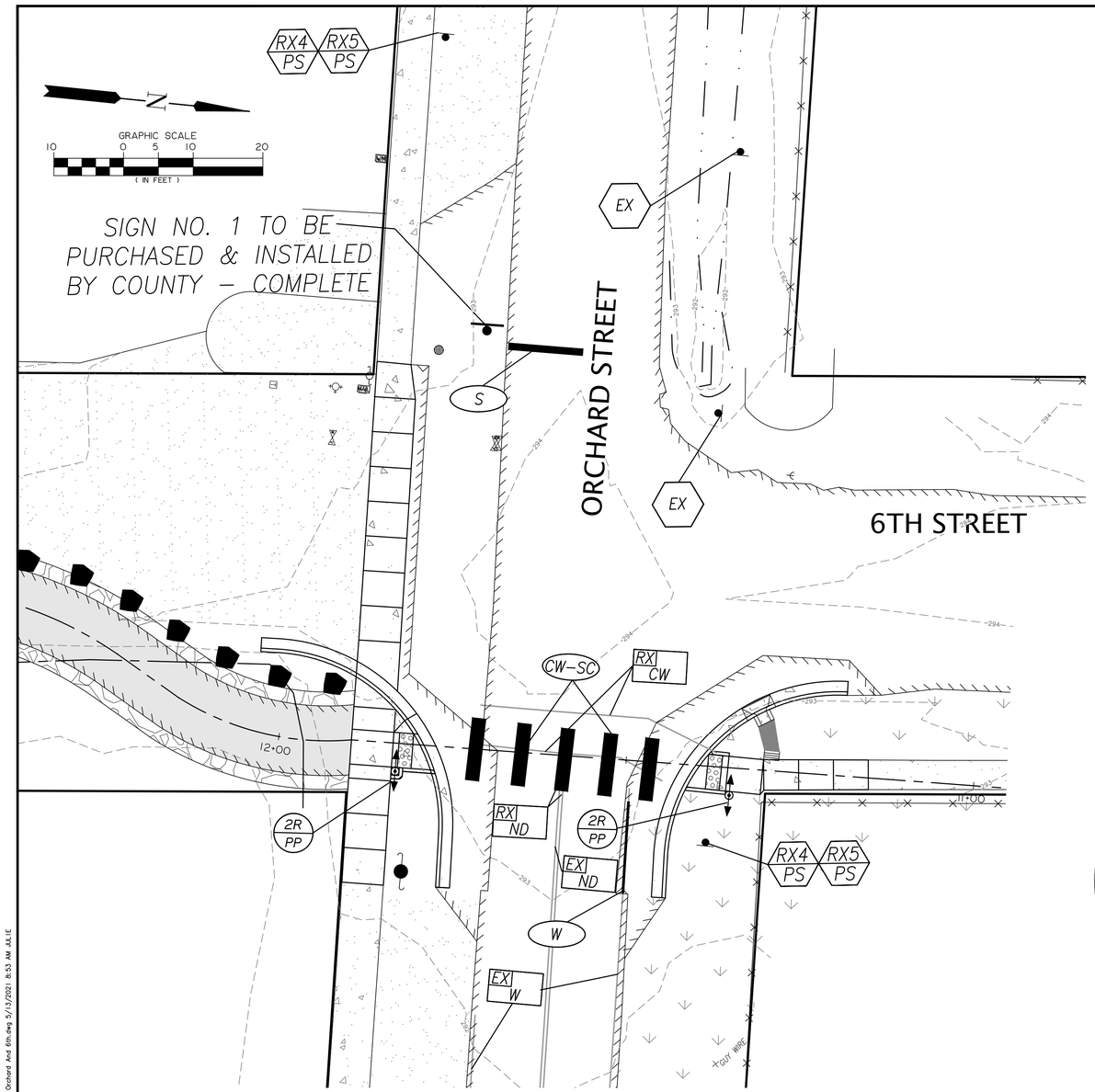
revisions:

date:	MAY 10, 2021
drawn by:	JLL
designer:	JLL
project no:	18-005E2

PROPOSED IMPROVEMENTS STA. 14+40 - 18+60

sheet:

C4



STRIPING NOTES:

- RX T — REMOVE STRIPING (T=TYPE) USING AN APPROVED METHOD PER CURRENT ODOT STANDARD SPECIFICATIONS.
- EX T — MAINTAIN AND PROTECT EXISTING STRIPING (T=TYPE)
- CW–SC — CONSTRUCT STAGGERED CONTINENTAL CROSSWALK PER ODOT STANDARD DRAWINGS TM503 AND TM530.
- S — CONSTRUCT (S) STANDARD 1'–0" STOP BAR PER ODOT STANDARD DRAWING TM503.
- W — INSTALL 4" WIDE WHITE LINE PER MANUFACTURER'S SPECIFICATIONS; SEE STANDARD DRAWING TM500.

ALL STRIPING MATERIALS SHALL COMPLY WITH CURRENT OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (2021 EDITION OR NEWER) AND SHALL BE INSTALLED PER CURRENT OREGON STANDARD DRAWINGS.

REFER TO STANDARD DRAWING TM530 FOR STRIPING INSTALLATION DETAILS.

ALL LONGITUDINAL MARKINGS SHALL BE EXTRUDED OR SPRAYED THERMOPLASTIC APPLIED USING METHOD AB.

TRANSVERSE MARKINGS AND LEGENDS SHALL BE TYPE B–HS PREFORMED FUSED THERMOPLASTIC FILM HIGH SKID.

SIGNING NOTES:

- EX — MAINTAIN AND PROTECT EXISTING SIGN AND SIGN SUPPORT
- RXN M — REMOVE EXISTING SIGN (N) AND (M) SIGN SUPPORT. RETURN TO BENTON COUNTY SIGN MAINTENANCE OFFICE.

N = SIGN NUMBER
M = MATERIAL
MATERIAL OPTIONS ARE
PS = 2" PERFORATED STEEL SQUARE TUBE

ALL NEW SIGNAGE SHALL MEET OREGON STANDARD SPECIFICATIONS AND SHALL BE COMPOSED OF PRODUCTS IDENTIFIED ON THE ODOT QUALIFIED PRODUCTS LIST (SHEETING, LEGENDS, ETC). SIGN PLACEMENT SHALL BE AT BACK OF WALK.

NEW AND RELOCATED SIGNAGE (POSTS) SHALL MAINTAIN A MINIMUM OF 4.0 FEET OF UNOBSTRUCTED HORIZONTAL CLEARANCE WITHIN THE SIDEWALK FOR ADA PASSAGE. NOTIFY ENGINEER OF CONFLICTS WHERE UNOBSTRUCTED HORIZONTAL CLEARANCE WILL BE LESS THAN 5'. ENGINEER TO COMMUNICATE WITH COUNTY REPRESENTATIVES FOR ADJUSTMENTS AND RELOCATIONS.

ALL NEW SIGNS SHALL BE MUTCD COMPLIANT AND SHALL BE CONSTRUCTED OF SHEET ALUMINUM PER SECTION 02910.10 OF THE CURRENT OREGON STANDARD SPECIFICATIONS (2021 OR NEWER).

NEW SIGN BACKGROUND SHEETING SHALL BE RETROREFLECTIVE ASTM TYPE III OR IV.

NEW SIGN LEGENDS SHALL BE NONREFLECTIVE NEW SCHOOL ZONE AREA SIGNS SHALL BE FLORESCENT YELLOW–GREEN (SIGNS 2, & 3)

S1–1

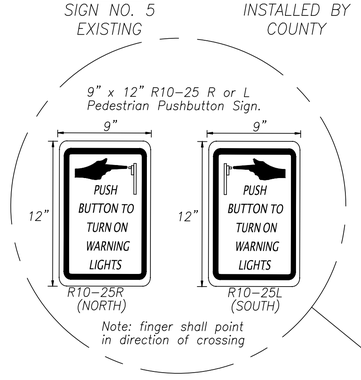
SIGN NO. 4 EXISTING

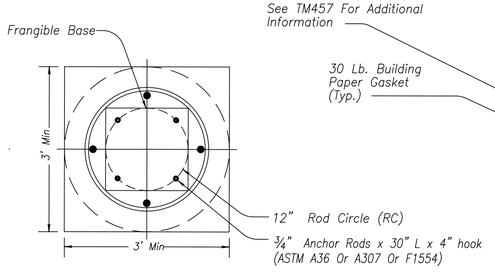
W16–7PL

SIGN NO. 5 EXISTING

R1–5b

SIGN NO. 1 NEW: 36"X36" TO BE PURCHASED AND INSTALLED BY COUNTY

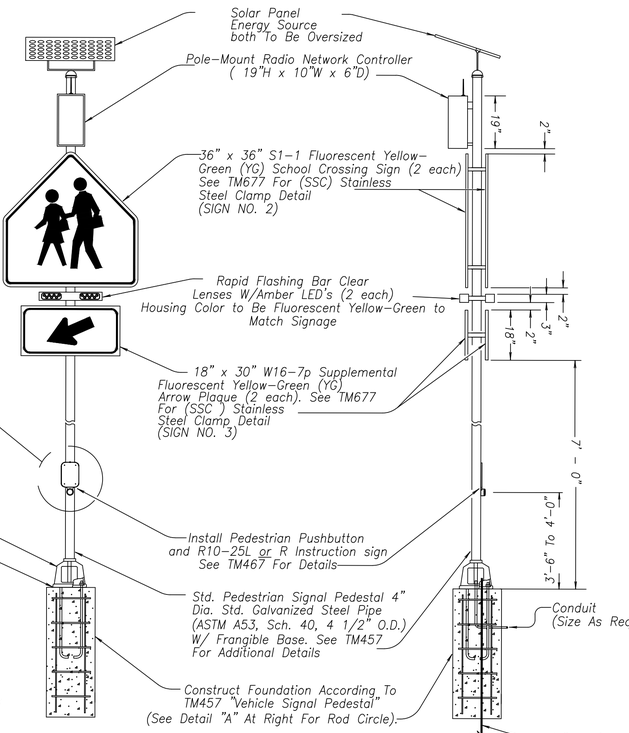




DETAIL "A".

NOTE: CONTACT BENTON COUNTY SIGNAL OPERATIONS ENGINEER TWO WEEKS PRIOR TO TURN ON FOR TIMING PARAMETERS.

ADD CONTROLLER CABINET DETAILS AS PER MANUFACTURER'S RECOMMENDATIONS



Front View — Side View

RRFB SIGNAL INSTALLATION NOTES:

XR PP — FURNISH AND INSTALL (X=NUMBER) SIDED FULLY FUNCTIONAL RECTANGULAR RAPID FLASHING BEACON (RRFB) SYSTEM ON PEDESTRIAN PEDESTAL WITH VEHICLE PEDESTAL FOUNDATION PER DETAIL, THIS SHEET, RRFB INSTALLATION SHALL HAVE ALL NECESSARY HARDWARE TO BE INSTALLED AS STAND–ALONE UNITS AND SHALL BE CAPABLE OF COMMUNICATING SIMULTANEOUS ACTIVATION AND DEACTIVATION BETWEEN INSTALLATIONS.

CONTROL SYSTEM:
PEDESTRIAN CROSSING SYSTEM INCLUDES CABINET, CABINET MOUNT, BATTERIES, CHARGE CONTROLLER, FLASHING UNIT (2 CIRCUIT, 50% DUTY, 70 FLASH/ MIN, SOLAR PANEL, SOLAR PANEL MOUNT, WIRING BETWEEN SOLAR PANEL AND CABINET, ALL EQUIPMENT SHALL BE ON THE ODOT QPL.

RECTANGULAR RAPID FLASHING BEACON (RRFB) SYSTEM PEDESTRIAN PEDESTAL INSTALLATION (TWO SIDED) TAPCO PART # 2180–RRFB XLS NOT TO SCALE

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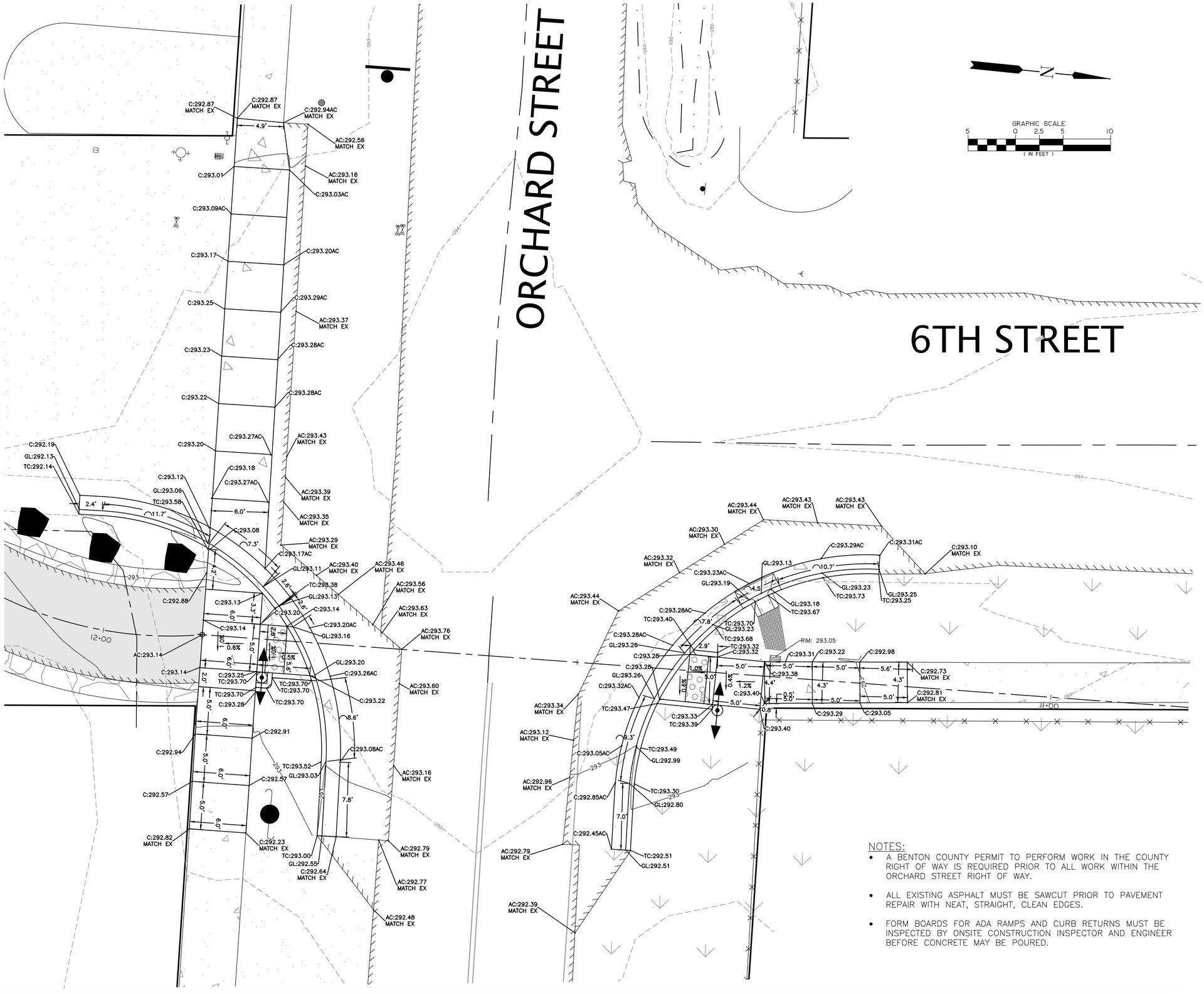
project no: 18-005E2

PROPOSED RRFB IMPROVEMENTS

sheet: C5



NOTES:

- A BENTON COUNTY PERMIT TO PERFORM WORK IN THE COUNTY RIGHT OF WAY IS REQUIRED PRIOR TO ALL WORK WITHIN THE ORCHARD STREET RIGHT OF WAY.
- ALL EXISTING ASPHALT MUST BE SAWCUT PRIOR TO PAVEMENT REPAIR WITH NEAT, STRAIGHT, CLEAN EDGES.
- FORM BOARDS FOR ADA RAMPS AND CURB RETURNS MUST BE INSPECTED BY ONSITE CONSTRUCTION INSPECTOR AND ENGINEER BEFORE CONCRETE MAY BE POURED.

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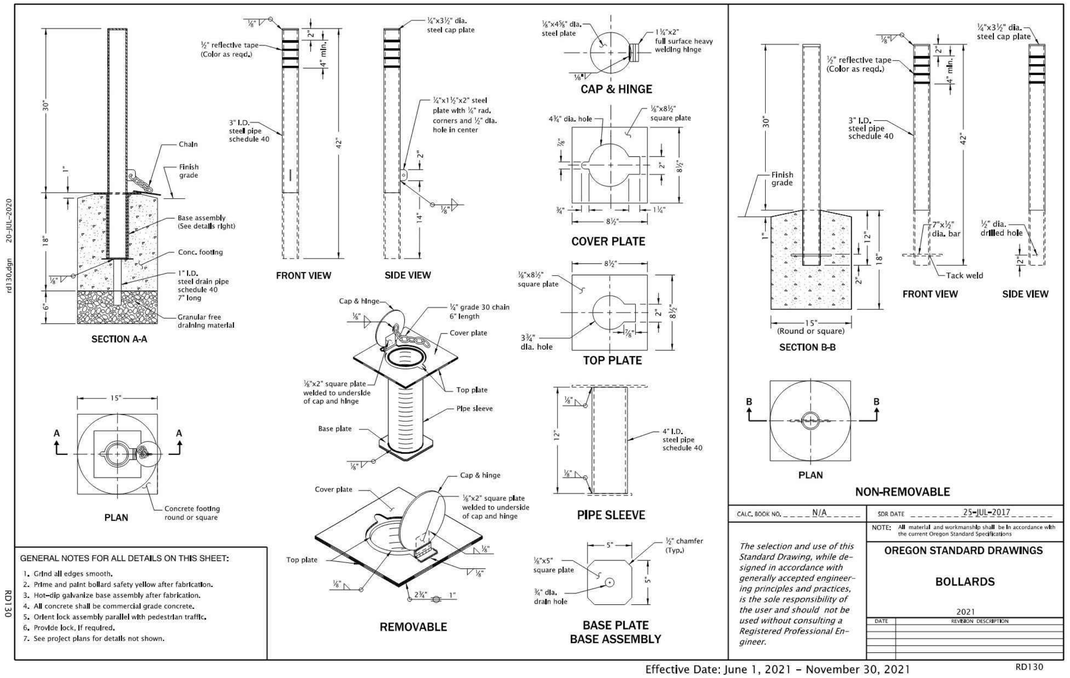
ORCHARD & 6TH STREET ADA RAMPS

sheet:

C6



GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:

1. Grind all edges smooth.
2. Prime and paint bollard safety yellow after fabrication.
3. Hot-dip galvanize base assembly after fabrication.
4. All concrete shall be commercial grade concrete.
5. Orient lock assembly parallel with pedestrian traffic.
6. Provide lock, if required.
7. See project plans for details not shown.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

CALC. BOOK NO. N/A

SDR DATE 25-JUL-2017

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

BOLLARDS

2021

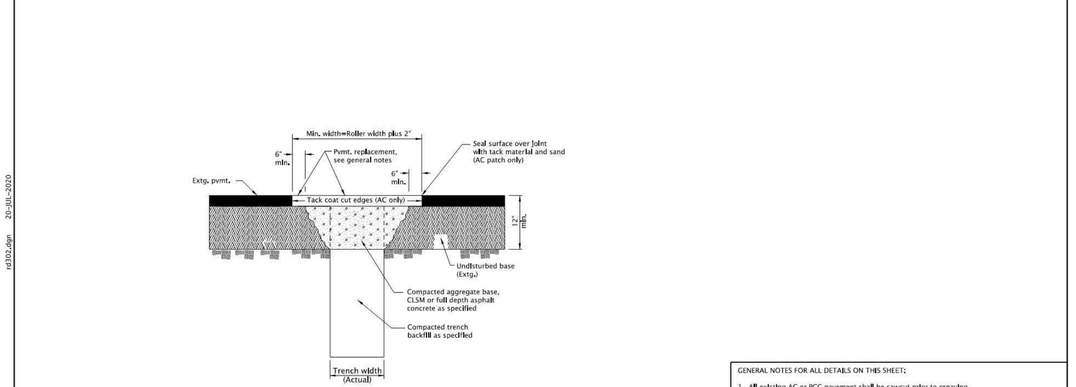
DATE	REVISION DESCRIPTION

RD130

rd130.dgn 20-JUL-2020

RD130

Effective Date: June 1, 2021 - November 30, 2021



GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:

1. All existing AC or PCC pavement shall be sawcut prior to repaving.
2. Concrete pavement shall be replaced with concrete to a minimum thickness of 8" or to the thickness of removed pavement, whichever is greater.
3. For joining new concrete to existing concrete, see contract plans for specific details.
4. Place AC mix minimum thkn. of 6" or the thkn. of the removed pavement, whichever is greater. Compact as specified.

CALC. BOOK NO. N/A

SDR DATE 20-JUL-2020

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

STREET CUT

2021

DATE	REVISION DESCRIPTION

RD302

rd302.dgn 20-JUL-2020

RD302

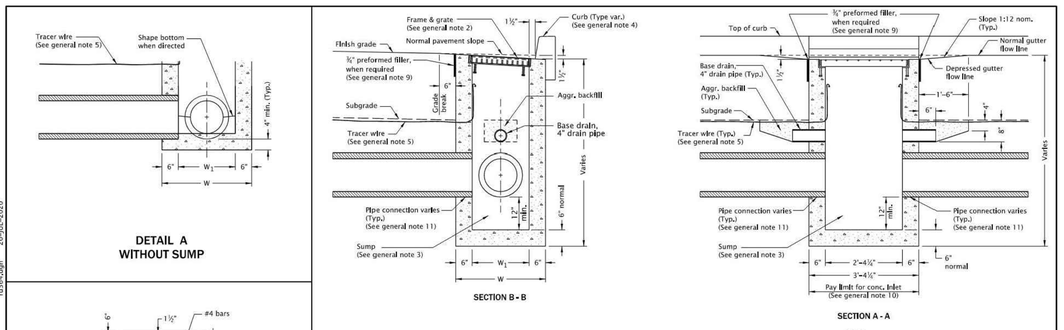


TABLE A		
INLET TYPE	W	W_1
G-1	2'-8¾"	1'-8¾"
G-2, G-2M, G-2MA	3'-3⅞"	2'-3⅞"

GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:

1. Where precast inlets are used as an alternate to cast-in-place inlets, a 4" compacted levelling bed of sand or ¾"-0 crushed aggregate shall be provided. All precast inlets shall conform to requirements of ASTM C913.
2. Graphics show G-1 inlet with Type 2 grate, See Table A for inlet dimensions. Type 1 grate allowed only in locations not subject to bicycle or pedestrian use. For frame and grate details, see Std. Dwg. RD365.
3. Provide sump only where shown on plans, and allowed by Jurisdiction. See Detail A for inlet without sump.
4. For curb details, see Std. Dwgs. RD700 & RD701.
5. See Std. Dwg. RD336 for tracer wire details, or approved alternate.
6. Max. pipe diameter varies with pipe material.
7. Location, elevation, diameter, slope, and number of pipe(s) varies, see project plans.
8. All concrete shall be commercial grade concrete.
9. ½" preformed filler (in concrete pavement or gutter only) to extend through thickness of concrete.
10. See Std. Dwg. RD363 for gutter transition section, when curb and gutter are required.
11. See Std. Dwg. RD339 for pipe to structure connections.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

CALC. BOOK NO. N/A

SDR DATE

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

CONCRETE INLETS TYPE G-1, G-2, G-2M, & G-2MA

2021

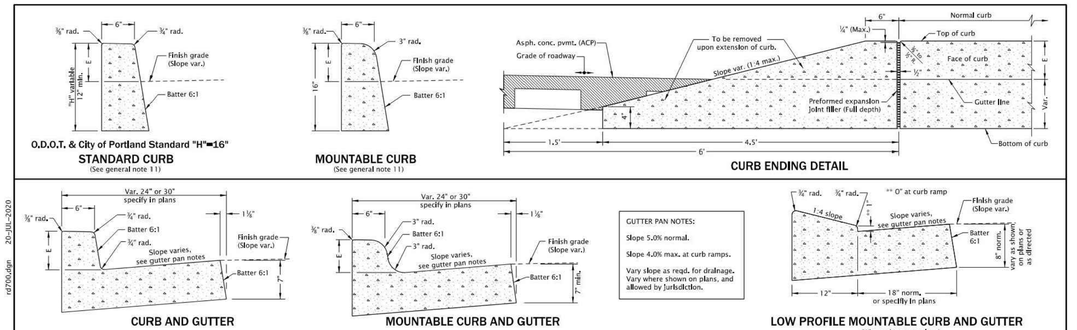
DATE	REVISION DESCRIPTION

RD364

rd364.dgn 20-JUL-2020

RD364

Effective Date: June 1, 2021 - November 30, 2021



GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:

1. Curb exposure "E" = 6" to 9", as measured vertically from flowline to highest point on curb. Vary as shown on plans or as directed. O.D.O.T. standard "E"=7".
2. Const. curb expansion joints at 200' maximum spacing, and at points of tangency, and at ends of each driveways.
3. Const. curb contraction joints at 15' maximum spacing, and at ends of each inlet and curb ramp.
4. Transitions shall be used to connect curbs of different exposures "E". ("E" is the total vertical dimension of those curb surfaces having a slope of 1:1 or steeper). Minimum desirable transition length shall be 20' for each 1" difference in "E".
5. Tops of all curbs shall slope toward the roadway at 1.5% max. (Max. 2.0% finished surface slope), unless otherwise shown, or as directed.
6. Dimensions are nominal, vary to conform with curb machine approved by the engineer.
7. Dimensions adjacent to radii are measured to the point of intersection of curb surfaces.
8. For sidewalk details, and monolithic curb & sidewalk, see Std. Dwgs. RD720 & RD721.
9. For drainage curbs, see Std. Dwg. RD701.
10. For curb ramp details, see Std. Dwgs. RD900 series.
11. On or along state highways, curb and gutter is required at curb ramp.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

CALC. BOOK NO. N/A

SDR DATE 20-JUL-2020

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

CURBS

2021

DATE	REVISION DESCRIPTION

RD700

rd700.dgn 20-JUL-2020

RD700

Effective Date: June 1, 2021 - November 30, 2021

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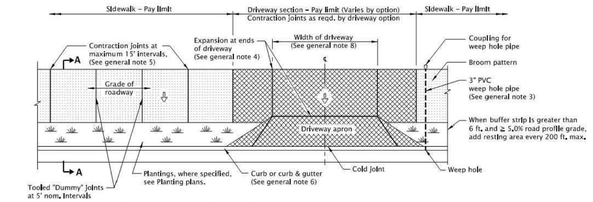
JLL
project no: 18-005E2

DETAILS

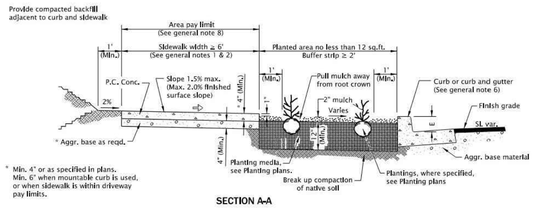
sheet: **C7**

2:\2018\18-005E Bailey Branch Trail\Drawings\Branch DWG For SRTS Grant\18-005E2 DETAILS Safe Routes To School - Orchard And 6th.dwg 5/10/2021 5:08 PM JILE

rd721.dgn 20-JUL-2020



TYPICAL PLAN VIEW - SEPARATED SIDEWALK



SECTION A-A

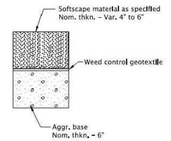
TYPICAL SETBACK SIDEWALK CROSS SECTION

L = curb exposure, see general note 6

GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:

1. Include additional paved or unpaved 2' shy distance to vertical faces higher than 5' such as retaining walls, sound walls, fences and buildings.
2. Curb type and sidewalk width as shown on plans or as directed. On sidewalks 8' and wider, provide a longitudinal joint at the midpoint.
3. Install 3" pvc weep hole pipes in sidewalks where shown on plans, and allowed by Jurisdiction. Place contraction joint over top of pipe, See Std. Dwg. RD700 for weep hole details.
4. Provide expansion joints around poles, posts, boxes, at ends of each driveway, and other fixtures which protrude through or against the structures. For sidewalk, monolithic curb & sidewalk, const. expansion joints at 45' maximum spacing. See Std. Dwg. RD722 for expansion joint details.
5. Const. contraction joints at 15' maximum spacing, and at ends of each curb ramp. See Std. Dwg. RD722 for contraction joint details.
6. Curb and gutter shown; see project plans for the curb design specified. For curb details, see Std. Dwgs. RD700 & RD701. ODOT standard E=7".
7. Sidewalk details are based on ODOT applicable standards.
8. Driveway encroaches into sidewalk shown; see project plans for the driveway design specified. For driveway details not shown, see Std. Dwgs. RD725, RD730, RD735, RD740, RD745 & RD750.
9. See project plans for details not shown.
10. Provide plantings in areas 12 SF or greater, as shown or directed. Treat areas less than 12 SF with mulch surfacing.

RD721



NON-PLANTED SOFTSCAPE CROSS SECTION

NOTES:
1. Use softscape materials allowed by Jurisdiction.
2. Approved softscape materials:
   a) Loose, durable round rock 2"–4"In diameter
   b) Lava rock 2"–4" diameter
   c) Wood chips/bark mulch
   d) Sand
3. No crushed aggregate or pea gravel allowed.
4. Install softscape material flush with the top of sidewalk.

LEGEND

Sidewalk pay limit.

Driveway pay limit, varies by option, (See general note 8).

Cross slope 1.5% max. (Max. 2.0% finished surface slope) (Normal sidewalk cross slope)

CALC. BOOK NO. N/A

SDR DATE 20-JUL-2020

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

OREGON STANDARD DRAWINGS

SEPARATED SIDEWALKS

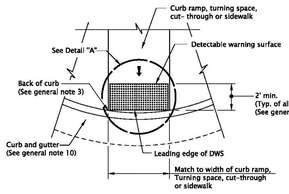
2021

DATE	REVISION DESCRIPTION

RD721

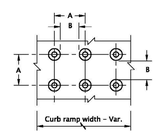
Effective Date: June 1, 2021 – November 30, 2021

rd902.dgn 20-JUL-2020

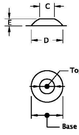


DETECTABLE WARNING SURFACE DETAIL

	A	B	C	D	E
MIN.	1.60"	0.65"	0.45"	0.90"	0.20"
MAX.	2.40"	—	0.91"	1.40"	0.20"

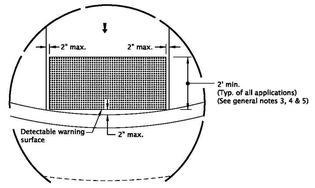


TRUNCATED DOME SPACING



TRUNCATED DOME

TRUNCATED DOME DETAILS



DETAIL "A"

GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:

1. Detectable warning surface details & locations are based on applicable ODOT Standards.
2. See project plans for details not shown. See Std. Dwgs. RD700 & RD701 for curbs.
3. The detectable warning surface shall extend the full width of the curb ramp opening, shared use path, blended transition, turning space, or other roadway entrance as applicable. A gap of up to 2 inches on each side of the detectable warning surface is permitted (measured at the leading edge of the detectable warning surface panel).
4. Detectable warning surface shall be placed at the back of curb for a minimum depth of 2 ft. in the direction of pedestrian travel at curb ramps that are adjacent to traffic. Detectable warning surface may be radial or rectangular, but must comply with the truncated dome size and spacing standards. Detectable warning surface may be cut to meet necessary shape as shown in plans. Detectable warning surface across a grade break is prohibited.
5. Color to be safety yellow if no color specified in construction note. Alternative colors require a design exception on or along state highways.
6. Detectable warning surface shall be used in the following locations:
   a) Curb ramps at street crossings.
   b) Crossing islands (Accessible Route Islands).
   c) Rail crossings.
7. Where public transportation stations (rail, bus, etc.) use platform boarding, detectable warning surface shall be placed along the full edge length of the station, when not protected by platform screens or guards, (see Std. Dwg. RD908).
8. Detectable warning surface shall not be used on the following locations:
   a) End of sidewalk transitions that are not at a crosswalk, (see Std. Dwgs. RD950, RD952 & RD960).
   b) Driveways, unless constructed with curb return or are signalized.
   c) Parking lots, access aisles and passenger loading zones where curb ramp does not lead to vehicular way.
9. Where no curb is present, the detectable warning surface shall be placed at the edge of the roadway.
10. On or along state highways, curb and gutter is required at curb ramps.

LEGEND:

Detectable warning surface

Cross slope 1.5% max. (Max. 2.0% finished surface slope) (Normal sidewalk cross slope)

Running slope 7.5% max. (Max. 8.3% finished surface slope)

CALC. BOOK NO. N/A

SDR DATE 20-JULY-2020

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

OREGON STANDARD DRAWINGS

DETECTABLE WARNING SURFACE DETAILS

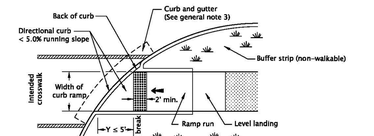
2021

DATE	REVISION DESCRIPTION
07-2020	DRAWING CREATED

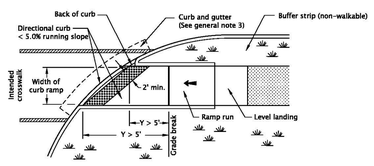
RD902

Effective Date: June 1, 2021 – November 30, 2021

rd905.dgn 20-JUL-2020

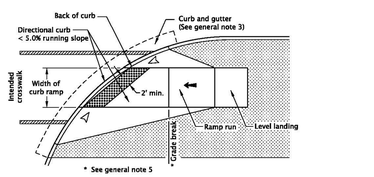


CURB RAMP CROSSING
GRADE BREAK ≤ 5 FT. FROM BACK OF CURB



CURB RAMP CROSSING
GRADE BREAK > 5 FT. FROM BACK OF CURB

RD905



CURB RAMP CROSSING
DIRECTIONAL CURB WITH FLARED CONSTRUCTION

GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:

1. Detectable warning surface details & locations are based on applicable ODOT Standards.
2. See project plans for details not shown. See Std. Dwgs. RD700 & RD701 for curbs. See Std. Dwg. RD902 for detectable warning surface installation details.
3. On or along state highways, curb and gutter is required at curb ramps.
4. Detectable warning surface placement for perpendicular ramps vary as shown.
5. Detectable warning surface placement across the grade break is prohibited.

LEGEND:

Marked or intended crossing location

Sidewalk

Detectable warning surface

Running slope 7.5% max. (Max. 8.3% finished surface slope)

Flare slope (Max. 10.0% finished surface slope)

CALC. BOOK NO. N/A

SDR DATE 20-JULY-2020

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

OREGON STANDARD DRAWINGS

DETECTABLE WARNING SURFACE PLACEMENT FOR DIRECTIONAL CURBS

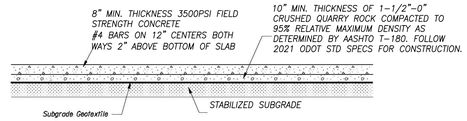
2021

DATE	REVISION DESCRIPTION
07-2020	DRAWING CREATED

RD905

Effective Date: June 1, 2021 – November 30, 2021

det1740.dgn 23-AUG-2006



The selection and use of this detail, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

OREGON DEPARTMENT OF TRANSPORTATION
TECHNICAL SERVICES
DETAILS

COMMERCIAL DRIVEWAY TYPICAL SECTION

DETAIL NO. DET1740

DET1740

Branch ENGINEERING Inc
Since 1977
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www.BranchEngineering.com
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REGISTERED PROFESSIONAL ENGINEER
#62780PE
DIGITALLY SIGNED
OREGON
MAY 13, 2005
DAMIEN GILBERT
EXPIRES: JUNE 30, 2021

SAFE ROUTES TO SCHOOL
MONROE GRADE SCHOOL

CITY OF MONROE, OREGON
PO BOX 486
MONROE, OREGON

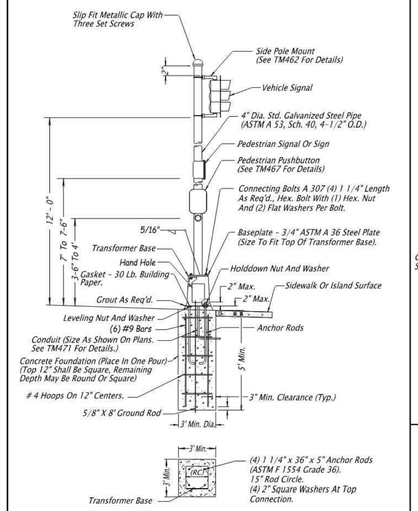
revisions:

date: MAY 10, 2021
drawn by: JLL
JLL
project no: 18-005E2

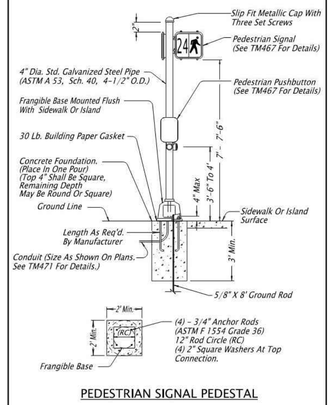
DETAILS

sheet: **C8**

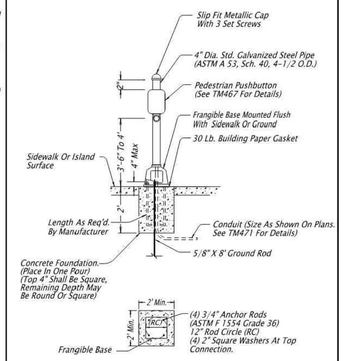
2:\2018\18-005E Bailey Branch Trail\Drawings\Branch DWG For SRTS Grant\18-005E2 DETAILS Safe Routes To School – Orchard And 6th.dwg 5/10/2021 5:08 PM JILIE



VEHICLE SIGNAL PEDESTAL



PEDESTRIAN SIGNAL PEDESTAL



PEDESTRIAN OR BICYCLE PUSHBUTTON POST

General Notes:

1. *All Bolts, Nuts And Washers Shall Conform To 02560.20 And Be Galvanized Steel According To 02560.40 Unless Noted Otherwise.*
2. *All Anchor Rods Shall Be Galvanized Steel Conforming To 02560.30.*
3. *All Pole Entrances Containing Wiring Shall Be Smooth.*
4. *Install 1/4" Thick Preformed Expansion Joint Filler Around Footing In Sidewalk Area As Per Tm653.*
5. *Top Of Foundations Shall Have 0" - 1/4 " Exposure Above Finish Grade.*
6. *Flat Side Of Foundation Should Line Up With Back Of Sidewalk.*

CALC. BOOK NO. _ N/A _

SDR REPORT DATE ~~6-10-2021~~ 7-12-2021

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

VEHICLE, PEDESTRIAN SIGNAL AND PUSHBUTTON MOUNTING OPTION DETAILS

2021

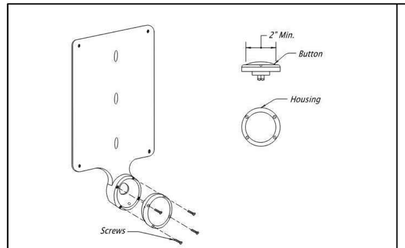
DATE	REVISION DESCRIPTION
01/21	Updated All Anchor Rod Details, Corrected Std, Dwg, Reference

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

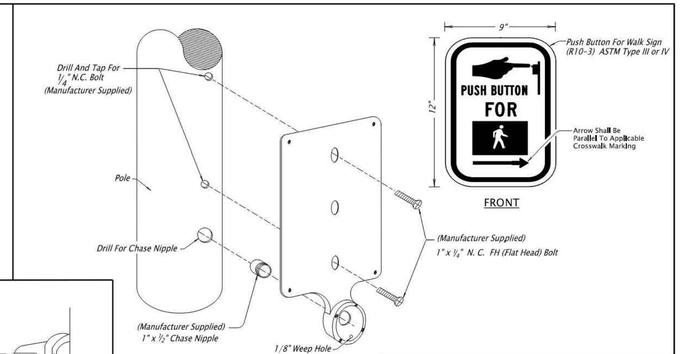
TM457

Effective Date: June 1, 2021 - November 30, 2021

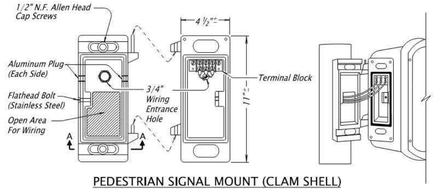
TM457



STANDARD PUSHBUTTON



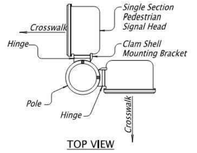
STANDARD PUSHBUTTON STATION AND INSTRUCTION SIGN



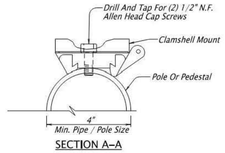
PEDESTRIAN SIGNAL MOUNT (CLAM SHELL)

NOTES:

1. *Where Two Heads Are Side Mounted On 4" Conduit, Proper Clearance Shall Be Maintained To Allow Legend To Be Fully Visible.*
2. *Clam Shells To Be Orientated So That The Heads Can Be Opened For Maintenance. (Verify Hinge Placement Of Clamshell).*



TOP VIEW



SECTION A-A

CLAM SHELL ORIENTATION

General Notes:

1. *All Screws, Bolts, Nuts And Washers Shall Be Type 304 Or 316 Stainless Steel Unless Noted Otherwise.*
2. *Bolts And Screws Shall Have Square Or Hex Heads. Allen Head Fasteners Not Allowed.*
3. *Drill And Tap Pole As Per Orientation Shown On Plans.*
4. *Horizontal Reach To The Pushbutton Shall Be 10 Inches Maximum. See Plans Or Consult Engineer To Ensure Compliance.*

CALC. BOOK NO. _ N/A _

SDR REPORT DATE 7-12-2021

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

PEDESTRIAN SIGNAL MOUNT AND PEDESTRIAN PUSHBUTTON DETAILS

2021

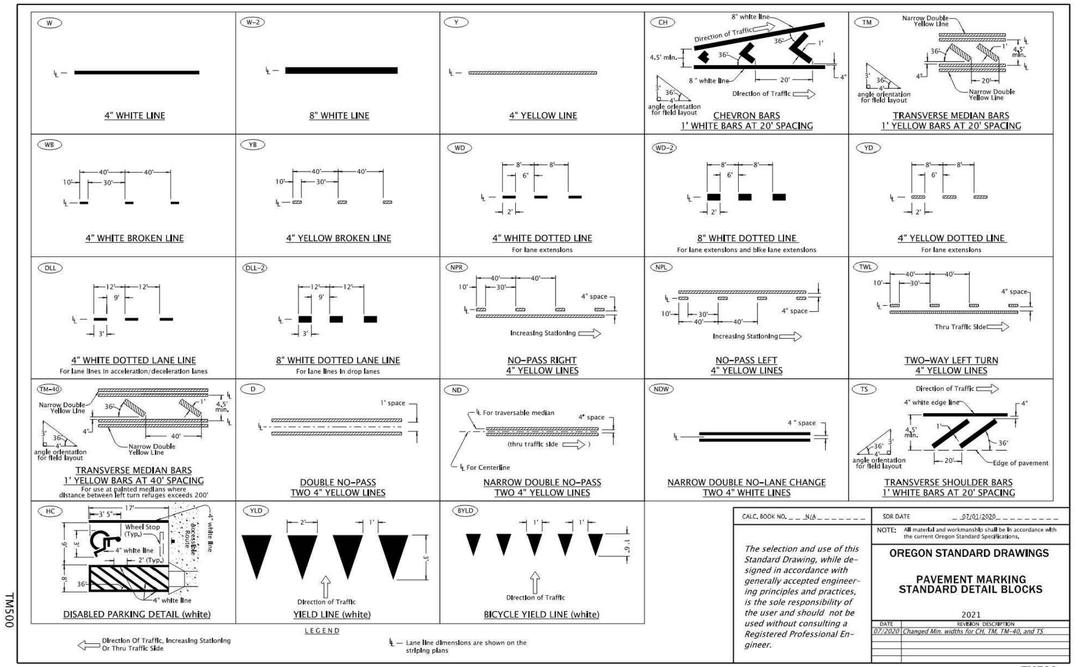
DATE	REVISION DESCRIPTION

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

TM467

Effective Date: June 1, 2021 - November 30, 2021

TM467



CALC. BOOK NO. _ N/A _

SDR REPORT DATE 7-12-2021

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS

PAVEMENT MARKING STANDARD DETAIL BLOCKS

2021

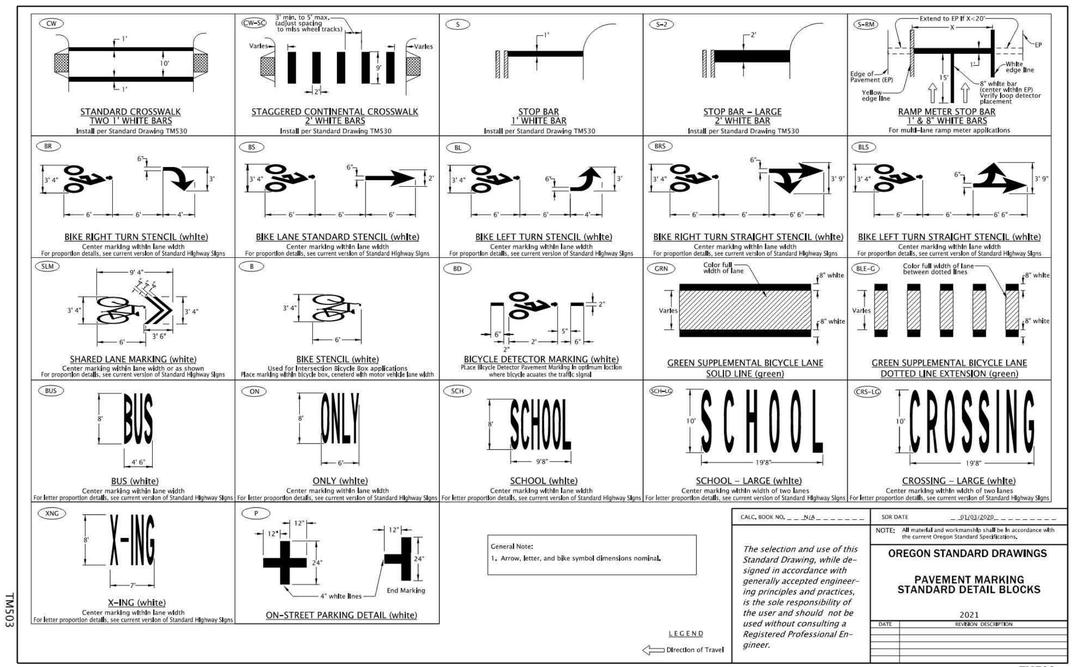
DATE	REVISION DESCRIPTION
07/2020	Changed Min. widths for CH, TM, TM-40, and TS

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

TM500

Effective Date: June 01, 2021 - November 30, 2021

TM500



CALC. BOOK NO. _ N/A _

SDR DATE 7-12-2021

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS

PAVEMENT MARKING STANDARD DETAIL BLOCKS

2021

DATE	REVISION DESCRIPTION

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

TM503

Effective Date: June 01, 2021 - November 30, 2021

TM503

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OREGON
MAY 13, 1994
DAMIEN GILBERT
EXPIRES: JUNE 30, 2021

SAFE ROUTES TO SCHOOL
MONROE GRADE SCHOOL

CITY OF MONROE, OREGON
PO BOX 486
MONROE, OREGON

revisions:

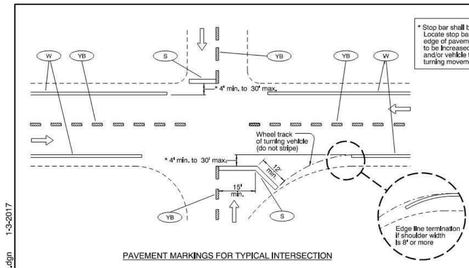
date: MAY 10, 2021
drawn by: JLL
JLL
project no: 18-005E2

DETAILS

sheet: C9

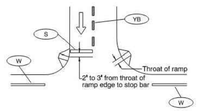
2:\2018\18-005E Bailey Branch Trail\Drawings\Branch DWG For SRTS Grant\18-005E2 DETAILS Safe Routes To School - Orchard And 6th.dwg 5/10/2021 5:08 PM JILE

TM530.dgn 1-3-2017

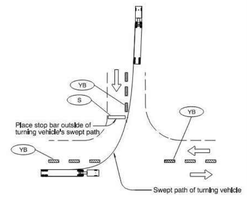


PAVEMENT MARKINGS FOR TYPICAL INTERSECTION

* Stop bar shall be placed as near as possible to the intersecting traveled way. Locate stop bar 4' min. to 30' max. in advance of the extended fog line, edge of pavement, or curb face. Minimum stop bar distance may need to be increased, depending on location of pedestrian ramps (see Detail "A") and/or vehicle turn radii (see Detail "B"). Field verify sight distance and truck turning movements.



Detail "A"
STOP BAR PLACEMENT WITH RESPECT TO PEDESTRIAN RAMPS



Detail "B"
STOP BAR PLACEMENT WITH RESPECT TO TURN RADII

INSTALLATION OF BIKE LANE STENCILS FOR BIKE LANE SEPARATED BY RIGHT TURN LANE

General Note:
Install bike lane stencil to avoid right turning vehicle wheel tracks.

INSTALLATION OF BIKE LANE STENCILS FOLLOWING INTERSECTIONS

STAGGERED CONTINENTAL LAYOUT

STANDARD CROSSWALK BARS AT INTERSECTION

General Notes:
1. Install crosswalk bars such that the throat of the ADA ramp is entirely within crosswalk markings, or 5' back of extended fog line, edge of pavement, or curb face.

LEGEND
⇦ Direction of Travel
L_t – Lane line dimensions are shown on the striping plans

To be accompanied by Standard Dwg. Nos. TM500 thru TM504

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

CALC. BOOK NO.: N/A
SDR DATE: July 10, 2020
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS
INTERSECTION PAVEMENT MARKINGS (CROSSWALK, STOP BAR & BIKE LANE STENCIL)
2021

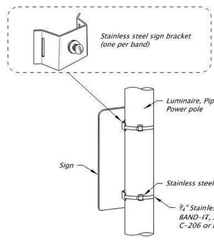
DATE	REVISION DESCRIPTION
7/10/20	Changed drawing reference

plotfile_TM200s_500s.dgn :: Default 1/19/2021 5:02:34 PM hwyr20m

Effective Date: June 1, 2021 - November 30, 2021

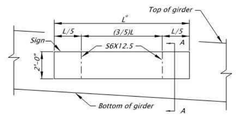
TM530

tm677.dgn 10-JUL-2020



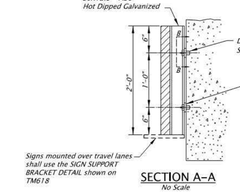
Signs mounted to vertical posts that use stainless steel clamps shall not be wider than 36". Use 2 clamps for all signs less than 48" in height and 3 clamps for signs 48" to 60" in height.

STAINLESS STEEL CLAMP (SSC) DETAIL
No Scale



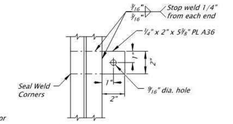
* – L maximum is 14'-0".

SIGN ELEVATION
No Scale



Signs mounted over travel lanes shall use the SIGN SUPPORT BRACKET DETAIL shown on TM618

SECTION A–A
No Scale



SECTION B–B
No Scale

Notes:
1. *Install resin bonded anchors according to Section 00535.*
2. *Resin bonded anchors shall conform to ASTM A307.*
3. *The hole depths shall develop the pullout strength specified in Table 00535-1.*
4. *Tighten 1/2" dia. anchors using 16 ft-lb of torque for waxed galvanized and 40 ft-lb of torque for galvanized only connections.*

ROAD NAME SIGN STRUCTURE MOUNT DETAIL

GENERAL NOTES

1. *For Secondary Sign Mounts See TM678.*

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

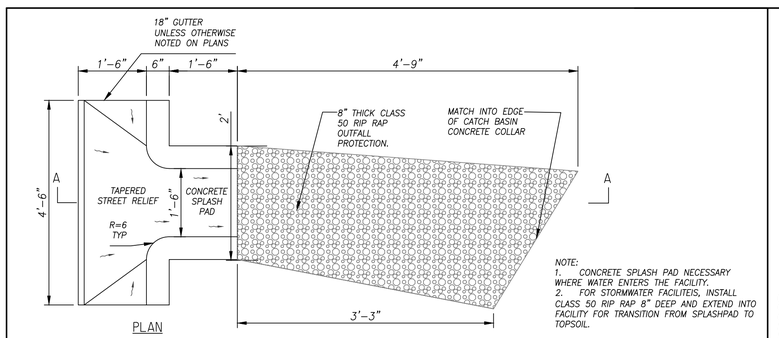
CALC. BOOK NO.:
SDR DATE: 06-JUL-2015
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS
SIGN MOUNTS
2021

DATE	REVISION DESCRIPTION

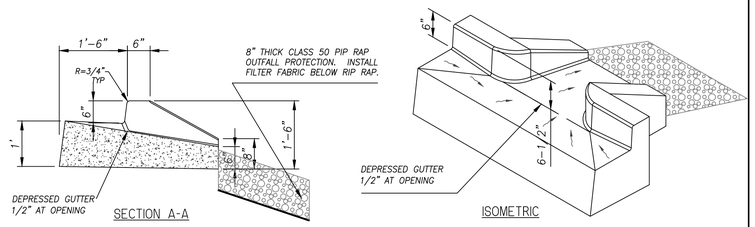
TM677

Effective Date: June 1, 2021 – November 30, 2021



PLAN

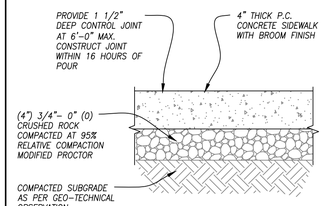
NOTE:
1. CONCRETE SPLASH PAD NECESSARY WHERE WATER ENTERS THE FACILITY.
2. FOR STORMWATER FACILITEIS, INSTALL CLASS 50 RIP RAP 8" DEEP AND EXTEND INTO FACILITY FOR TRANSITION FROM SPLASHPAD TO TOPSOIL.



SECTION A-A

ISOMETRIC

1 *CURB CUT SPILLWAY*
SCALE: N.T.S.



2 *CONCRETE SIDEWALK*
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revisions:

date: MAY 10, 2021
drawn by: JLL
JLL
project no: 18-005E2

DETAILS

sheet: **C10**

2:\20\R\18-005E Bailey Branch Trail\Drawings\Branch DWG For SRTS Grant\18-005E2 DETAILS Safe Routes To School - Orchard And 6th.dwg 5/10/2021 5:08 PM JLLE